



# **City of Vancouver** *Zoning and Development By-law*

Community Services, 453 W. 12th Ave Vancouver, BC V5Y 1V4 ☎ 604.873.7344 fax 873.7060

[planning@city.vancouver.bc.ca](mailto:planning@city.vancouver.bc.ca)

## **CD-1 (171)**

*3003 Kingsway*

*5250 Rupert Street*

*By-law No. 5890*

*(Being a By-law to Amend By-law 3575, being the Zoning and Development By-law)*

***Effective April 2, 1985***

*(Amended up to and including By-law No. 8169, dated March 14, 2000)*

BY-LAW NO. 5890

A By-law to amend By-law No. 3575, being the Zoning and Development By-law

THE COUNCIL OF THE CITY OF VANCOUVER, in open meeting assembled, enacts as follows:

1 The “Zoning District Plan” annexed to By-law No. 3575 as Schedule “D” is hereby amended according to the plan marginally numbered Z-303b and attached to this By-law as Schedule “A”, and in accordance with the explanatory legends, notations and references inscribed thereon, so that the boundaries and districts shown on the Zoning District Plan are varied, amended or substituted to the extent shown on Schedule “A” of this By-law, and Schedule “A” of this By-law is hereby incorporated as an integral part of Schedule “D” of By-law No. 3575.

2 Uses

The area shown included within the heavy black outline on Schedule “A” is rezoned to CD-1, and the only uses permitted within the area, subject to such conditions as Council may by resolution prescribe, and the only uses for which development permits will be issued are -

- (a) general business commercial uses, subject to the following:
  - (i) all uses shall be of a type requiring a central location to serve large neighbourhoods as distinct from a convenience commercial use catering primarily to the needs of a local neighbourhood;
  - (ii) all uses shall be located on the southerly portion of the site and shall be oriented towards Kingsway.
- (b) a maximum of 45 townhouses and apartments, subject to the following:
  - (i) all units shall be developed on the northerly portion of the site away from the Kingsway frontage;
- (c) accessory buildings and accessory uses customarily ancillary to the above.

3 Floor Space Ratio

3.1 The floor space ratio shall not exceed 0.80, comprising a maximum floor space ratio of 0.20 for the commercial uses, computed in accordance with the provisions of the C-2 District Schedule, and a maximum floor space ratio of 0.60 for the residential uses, computed in accordance with the provisions of the RT-2A District Schedule.

3.2 For the purpose of computing floor space ratio the area of the site shall be all that area shown within the solid black lines on Diagram I in Section 6, below.

3.3 Where exterior walls greater than 152 mm in thickness have been recommended by a Building Envelope Professional as defined in the Building By-law, the area of the walls exceeding 152 mm, but to a maximum exclusion of 152 mm thickness, shall be excluded in the computation of floor space ratio, except that this section shall not apply to walls in existence prior to March 14, 2000. [8169; 00 03 14]

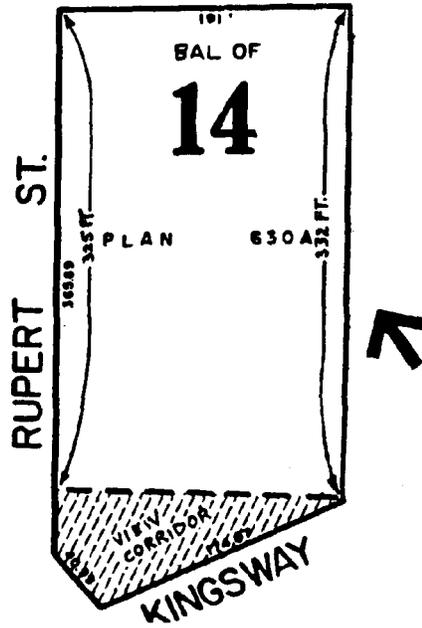
4 Site Coverage

The maximum site coverage for all buildings, surface parking areas, carports, maneuvering aisles, driveways, loading facilities and all other vehicle facilities shall be 48 percent of the site area.

**Note:** Information included in square brackets [ ] identifies the by-law numbers and dates for the amendments to By-law No. 5890 or provides an explanatory note.

- 5 Height  
The maximum height of a building, measured in accordance with the applicable provisions of the Zoning and Development By-law, shall be 12.192 m (40.00 ft.).
- 6 Off-street Parking  
Off-street parking and loading spaces for all uses shall be provided, developed and maintained in accordance with applicable provisions of Section 12 of the Zoning and Development By-law, except as follows:
- (a) all off-street parking ancillary to the residential uses shall be located underground;
  - (b) any parking located within the area identified as "view corridor" in Diagram 1 below shall be depressed below the elevation of Kingsway.

Diagram 1



- 7 This By-law comes into force and takes effect on the date of its passing.

DONE AND PASSED in open Council this 2nd day of April, 1985.

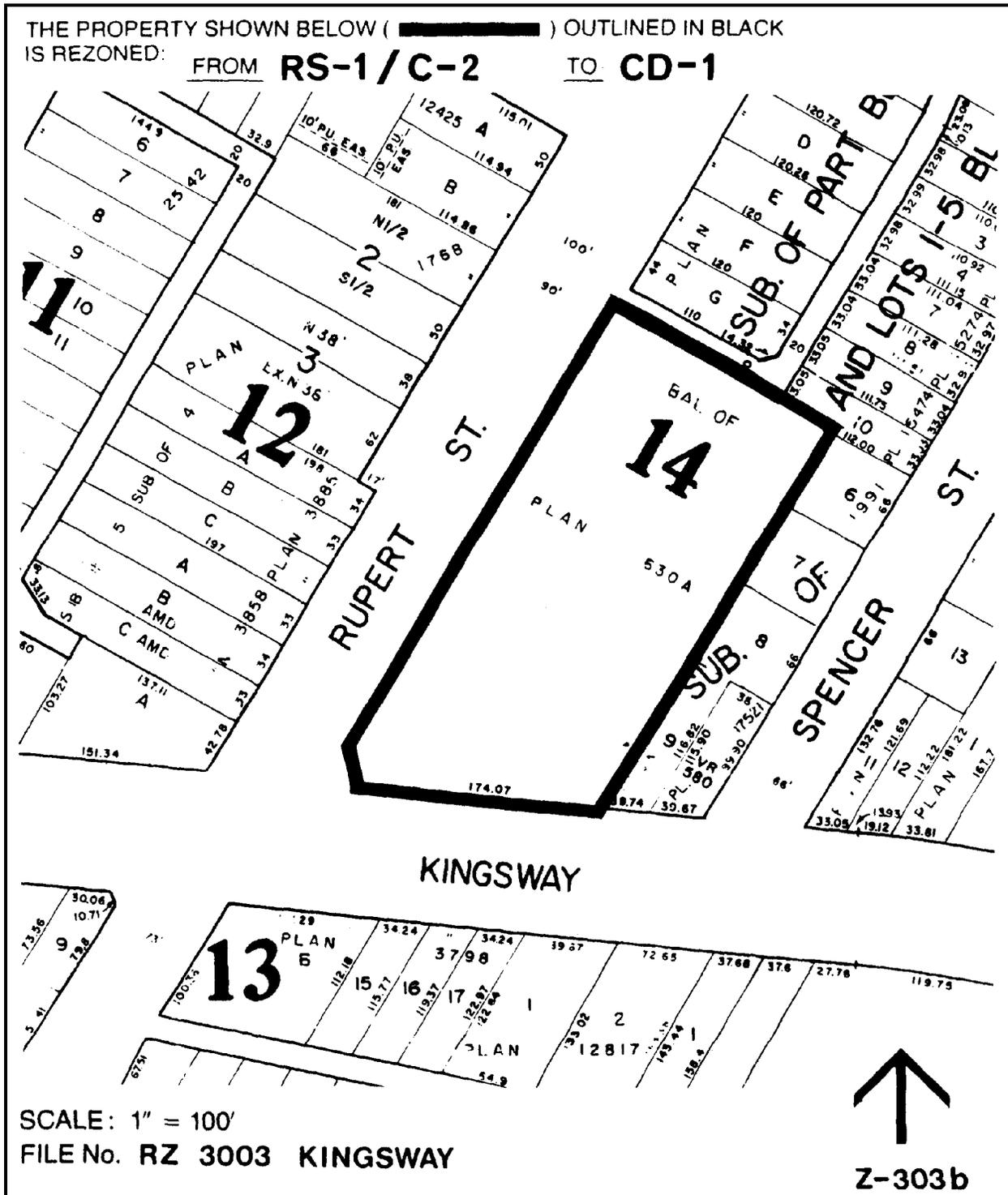
(signed) Michael Harcourt  
Mayor

(signed) R. Henry  
City Clerk

"I hereby certify that the foregoing is a correct copy of a By-law passed by the Council of the City of Vancouver on the 2nd day of April, 1985, and numbered 5890.

CITY CLERK"

By-law No. 5890 being a By-law to amend By-law No. 3575, being the Zoning and Development By-law



CITY OF VANCOUVER  
MEMORANDUM

171

From: CITY CLERK

Date: APRIL 4, 1985

To:  DIRECTOR OF PLANNING  
Attn: Zoning Planner

Refer File:

<b>RECEIVED</b>	
CITY	
APR 10 1985	
NO. <i>C-1617</i>	
REF. <i>Dmed</i>	
COPY TO .....	
ANSWER REQ'D .....	

Subject: Motion re Proposed Conditions for Development  
(3003 Kingsway)

I wish to advise you of the attached extract from the Minutes of the Vancouver City Council meeting of April 2, 1985.



CITY CLERK

Att.  
RH:ss

EXTRACT FROM THE MINUTES OF THE VANCOUVER CITY COUNCIL MEETING  
HELD APRIL 2, 1985

---

A. Proposed Conditions for Development  
(3003 Kingsway)

MOVED by Ald. Brown,  
SECONDED by Ald. Ford,

THAT the approved form of development be generally as presented in the drawings prepared by Derek Neale, Architects, stamped "Received, City Planning Department, May 24, 1984", provided that the Director of Planning may allow minor alterations to this approved form of development when approving the detailed scheme of development;

AND FURTHER THAT this site be brought to the attention of the Planning and Development Committee of Council if the proposed development has not been started within one (1) year from the date of enactment of the proposed by-law.

- CARRIED UNANIMOUSLY

# 171

3003 Kingsway

3003 Kingsway

BY-LAW NO. 5890

A By-law to amend the  
Zoning and Development By-law,  
being By-law No. 3575

THE COUNCIL OF THE CITY OF VANCOUVER, in open meeting  
assembled, enacts as follows:

1. The "Zoning District Plan" annexed to By-law No. 3575 as Schedule "D" is hereby amended according to the plan marginally numbered Z-303b and attached to this By-law as Schedule "A", and in accordance with the explanatory legends, notations and references inscribed thereon, so that the boundaries and districts shown on the Zoning District Plan are varied, amended or substituted to the extent shown on Schedule "A" of this By-law, and Schedule "A" of this By-law is hereby incorporated as an integral part of Schedule "D" of By-law No. 3575.

2. USES

The area shown included within the heavy black outline on Schedule "A" is rezoned to CD-1, and the only uses permitted within the area, subject to such conditions as Council may by resolution prescribe, and the only uses for which development permits will be issued are -

- (a) general business commercial uses, subject to the following:
  - (i) all uses shall be of a type requiring a central location to serve large neighbourhoods as distinct from a convenience commercial use catering primarily to the needs of a local neighbourhood;
  - (ii) all uses shall be located on the southerly portion of the site and shall be oriented towards Kingsway.
- (b) a maximum of 45 townhouses and apartments, subject to the following:
  - (i) all units shall be developed on the northerly portion of the site away from the Kingsway frontage;
- (c) accessory buildings and accessory uses customarily ancillary to the above.

3. FLOOR SPACE RATIO

3.1 The floor space ratio shall not exceed 0.80, comprising a maximum floor space ratio of 0.20 for the commercial uses, computed in accordance with the provisions of the C-2 District Schedule, and a maximum floor space ratio of 0.60 for the residential uses, computed in accordance with the provisions of the RT-2A District Schedule.

3.2 For the purpose of computing floor space ratio the area of the site shall be all that area shown within the solid black lines on Diagram 1 in Section 6, below.

4. SITE COVERAGE

The maximum site coverage for all buildings, surface parking areas, carports, maneuvering aisles, driveways, loading facilities and all other vehicle facilities shall be 48 percent of the site area.

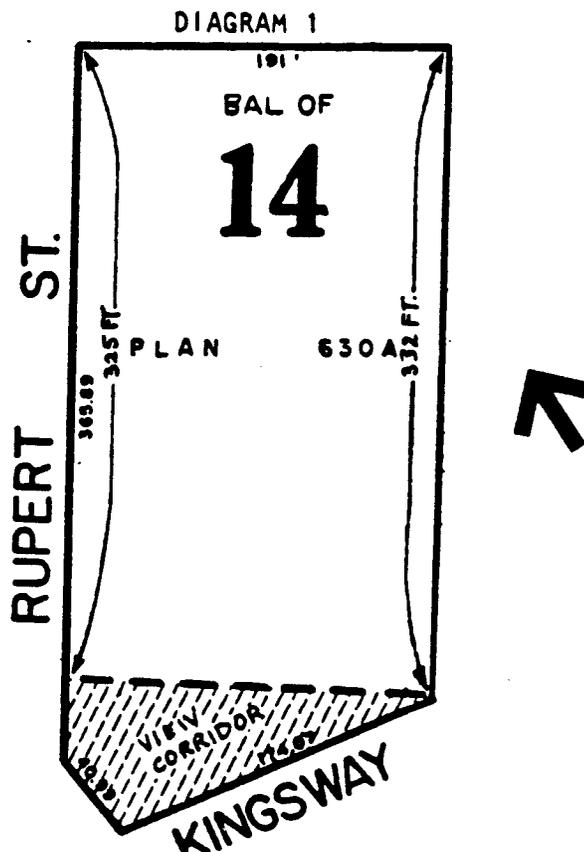
5. HEIGHT

The maximum height of a building, measured in accordance with the applicable provisions of the Zoning and Development By-law, shall be 12.192 m (40.00 ft.).

6. OFF-STREET PARKING

Off-street parking and loading spaces for all uses shall be provided, developed and maintained in accordance with applicable provisions of Section 12 of the Zoning and Development By-law, except as follows:

- (a) all off-street parking ancillary to the residential uses shall be located underground;
- (b) any parking located within the area identified as "view corridor" in Diagram 1 below shall be depressed below the elevation of Kingsway.



7. This By-law comes into force and takes effect on the date of its passing.

DONE AND PASSED in open Council this 2nd day of  
April , 1985.

(signed) Michael Harcourt  
Mayor

(signed) R. Henry  
City Clerk

"I hereby certify that the foregoing is a correct copy of a By-law passed by the Council of the City of Vancouver on the 2nd day of April, 1985, and numbered 5890.

CITY CLERK"



CD-1 (171)

TO: ADDRESS

CITY OF VANCOUVER  
MEMORANDUM

From: CITY CLERK

Date: 25th July 1984

To: CITY MANAGER  
DIRECTOR OF PLANNING  
ASSOCIATE DIRECTOR - ZONING  
CITY ENGINEER  
URBAN DESIGN PANEL  
CLERK, PUBLIC HEARING

Refer File: 5305

Subject: ZONING APPLICATION: 3003 KINGSWAY (AT RUPERT)

---

Please be advised that City Council at its meeting on Tuesday, July 24, 1984, approved the recommendation of the City Manager as contained in his attached report dated July 20, 1984, with regard to the above matter.

CITY CLERK

CLevine:mfm  
Att.

Also sent to:

Messrs. Meale & Staniszki, Architects, 503 - 321 Water Street V6B 1B8

2. Rezoning Application -  
3003 Kingsway (at Rupert)

The Director of Planning reports as follows:

PURPOSE

This report is an assessment of an application received from Neale and Staniszki Architects, requesting to rezone 3003 Kingsway (Balance of Block 14, D.L. 37, Plan 630A) as follows:

- PRESENT ZONING: RS-1 One-Family Dwelling District  
C-2 Commercial District
- REQUESTED ZONING: CD-1 Comprehensive Development District
- STATED PURPOSE: Constructing a mixed-use residential/commercial development containing 45 dwelling units and 1 312 m<sup>2</sup> (14,120 sq. ft.) of commercial floor space.

This application supersedes a previous application for CD-1 from Derek Neale and Associates Architects, which received Council approval in principle in April 1982.

This scheme generally complies with the Kingsway Task Force Guidelines but differs in the increase in commercial floor space from 376 m<sup>2</sup> (3,974 sq. ft.) to 1,312 m<sup>2</sup> (14,120 sq. ft.); the decrease in the residential floor space from 4,853 m<sup>2</sup> (52,244 sq. ft.) to 3,935 m<sup>2</sup> (42,360 sq. ft.); and the decrease in residential density from 37 units per acre (60 units) to 28 units per acre (45 units).

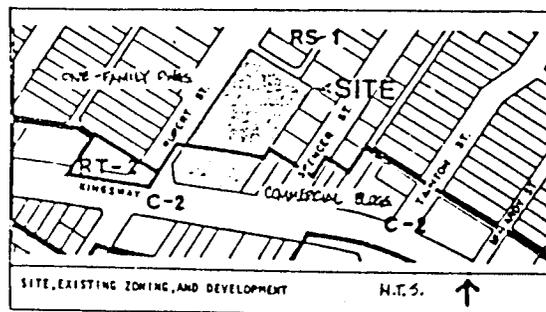
This scheme addresses three previous concerns of Council. Vehicle access has been relocated from the lane to Rupert Street; the emergency vehicle lane has been eliminated by the inclusion of sprinkler systems in the easterly buildings; and noise abatement design measures have been introduced for the residential units.

The Director of Planning is generally supportive of this scheme, but recommends design modifications to the commercial component and parking area.

SITE, EXISTING ZONING, AND DEVELOPMENT

The site is located on the northeast corner of Kingsway and Rupert Streets and has a total area of 0.656 ha. (1.62 ac.). See Diagram 1.

DIAGRAM 1



2.

Clause No. 2 Continued

The site is bounded by Kingsway, a major arterial on the south, Rupert Street, a minor arterial on the west, and a local lane on the north. Lands to the northeast, north and northwest of the site are zoned and developed with one- and two-storey, one-family dwellings in good condition. Parcels directly east of and abutting the site are shallow, maintaining depths of 20.117 m (66.00 ft.) with sub-standard rear yards.

The site falls from the southeast to the northwest at approximately an eight percent slope and offers views to the North Shore mountains and downtown.

BACKGROUND

There have been several rezoning initiatives over the years, the most recent being in 1977 when Council established the Kingsway Task Force, comprised of civic staff and local citizens, to formulate guidelines for four significant sites in the Kingsway-East area, one of these being the site in question.

The general concept for this site, as set out in the Guidelines adopted by Council in 1973, included a motel or low-rise, medium-density residential development, with some general business commercial uses fronting on Kingsway. Specific conditions for development included a suggested FSR for mixed commercial/residential use over the whole site of 0.75, limiting the commercial component to an FSR of 0.20. A maximum overall net residential density of 86.6 units per hectare (35 units per acre) was called for. In order to preserve the panoramic view of the downtown from Kingsway, provision for a westerly-oriented view corridor along the Kingsway frontage was also suggested.

In May 1981, a permit was issued to demolish the existing buildings on the site as the result of an order to demolish from the City Building Inspector.

The April, 1982 rezoning to CD-1, which was approved in principle only, consisted of 39 apartments and 21 townhouses in three building groups, three storeys in height plus basement, with a 370 m<sup>2</sup> (3,974 sq. ft.) commercial building one-storey in height. The residential FSR was 0.74, the commercial, 0.06, and the overall FSR was 0.60. The net residential density was 91 units per hectare (37 units per acre).

PROPOSED DEVELOPMENT

The present scheme consists of 45 stacked townhouses, generally three storeys in height plus basement, with 1 311.8 m<sup>2</sup> (14,120 sq. ft.) of commercial in two buildings one to two storeys in height. The proposed residential FSR is 0.60, the commercial, 0.20, and the overall FSR is 0.80. The net residential density is 69 units per hectare (28 units per acre).

Clause No. 2 Continued

The following table compares the two proposals:

	SCHEME APPROVED BY COUNCIL ON APRIL 1st, 1982	CURRENT CD-1 PROPOSAL
USE	Mixed commercial/residential use with 39 apartment dwelling units, 21 townhouse dwelling units, and 3,374 sq.ft. of commercial floor space.	Mixed commercial/residential use with 45 stacked townhouse units and 14,120 sq.ft. of commercial floor space.
DENSITY	37 units/acre (for residential)	28 units/acre (for residential)
FLOOR SPACE RATIO	Commercial 0.06 Residential 0.78 0.80 (before dedication) 0.85 (after dedication)	Commercial 3.20 Residential 6.60 0.80 (before dedication) 0.85 (after dedication)
SITE COVERAGE	42%	48%
HEIGHT	Maximum: 12.192 m (40.00 ft.)	Maximum: 12.192 m (40.00 ft.)
OFF-STREET PARKING	Commercial (surface) 9 spaces Residential (underground) 67 spaces Total of 76 spaces (Section 12 minimum requirement) Surface parking depressed below Kingsway elevation.	Commercial (surface) 25 spaces Residential (underground) 52 spaces Total of 77 spaces (Section 12 minimum requirement) Surface parking depressed below Kingsway elevation.
SETBACKS	Kingsway 1.5 m (4.92 ft.) Rupert 7.0 m (22.97 ft.) Northerly 7.5 m (24.61 ft.) (to centre line of lane) Easterly 9.0 m (29.53 ft.)	Kingsway - Main wall of building to a line perpendicular to the south-east corner of the site. Rupert - 8.0 m (26.24 ft.) residential 3.05 m (10.0 ft.) commercial Northerly - 7.5 m (24.61 ft.) (to centre line of lane) Easterly - 9.0 m (29.53 ft.) residential 0.0 m (0 ft.) commercial

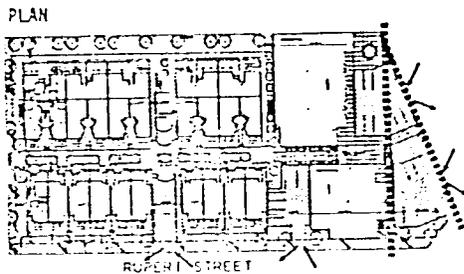
The stacked townhouses have individual entries from exterior walkways. Common hallways, elevator, and exit stairs contained in the previous scheme have been eliminated. All parking is contained in a secure underground garage with access from Rupert.

The present scheme maximizes the amount of commercial floor space under the Guidelines. The maximum building height is three storeys. Surface parking is provided for the commercial space and is depressed below the Kingsway elevation in compliance with the Guidelines and the identified view corridor is respected.

4

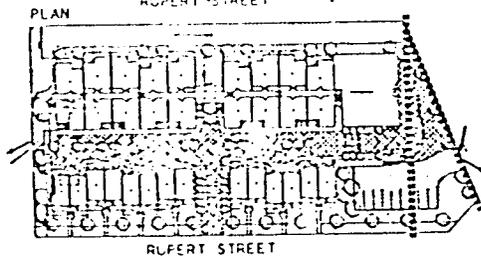
Clause No. 2 Continued

The following sketch plans and elevations compare the two proposals:



Commercial F.S.R. 0.20  
Residential F.S.R. 0.60  
Site Coverage 48%  
View Corridor ■■■■■

PRESENT SCHEME



Commercial F.S.R. 0.06  
Residential F.S.R. 0.74  
Site Coverage 42%  
View Corridor ■■■■■

PREVIOUS SCHEME

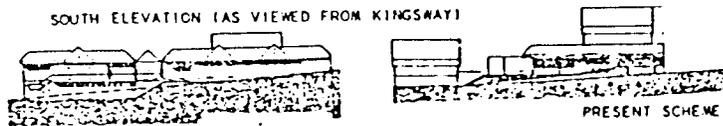
WEST ELEVATION (AS VIEWED FROM RUPERT)



PRESENT SCHEME

PREVIOUS SCHEME

SOUTH ELEVATION (AS VIEWED FROM KINGSWAY)



PREVIOUS SCHEME

PRESENT SCHEME

5

Clause No. 2 Continued

COMMENTS FROM REVIEWING AGENCIES

Urban Design Panel

At its meeting of June 6, 1984, the Urban Design Panel supported this application with the following concerns:

"The Panel felt that the definition of a positive streetscape was far more important than carving the whole front of the site off as a view corridor. They suggested that the building to the east be brought forward to align with Kingsway, with no parking in front of it. The building to the west should be reduced to a single functioning level of commercial use rather than the contrived two-storey attempt. The very corner of Rupert and Kingsway should be developed into a hard surfaced forecourt with attractive landscape elements. (The sloped street front surface parking was most inappropriate.)

The pockets of unconnected parking surrounding the commercial uses pose many problems, both visual and functional. All parking should be removed from the Kingsway frontage and instead, the Panel suggests combining the parking areas at the lower level, with access on Rupert Street, feeding back to Kingsway via a loop around the east end of the site."

City Engineer

In a memorandum dated June 1, 1984, the City Engineer commented as follows:

"This rezoning proposal is acceptable to the Engineering Department, provided the following matters are resolved as conditions of rezoning approval:

- arrangements satisfactory to the City Engineer and the Director of Legal Services are to be made for the dedication of the westerly 10 feet for road;
- the development permit application, when submitted, is to comply fully with the (parking and loading) provisions of Section 12 of the Zoning and Development By-law.

Alterations to the parking areas and the provision for only one commercial parking ingress/egress point is recommended."

ANALYSIS AND ASSESSMENT

Comparison of the Present and Previous Schemes

The present rezoning scheme reduces the amount of residential and increases the amount of commercial, with a modest increase in site coverage.

Overall floor space ratio, height and setbacks are generally consistent with the earlier submission.

Parking complies with Section 12 of the Zoning and Development By-law and all commercial surface parking is depressed below the Kingsway elevation in accordance with the Guidelines.

The present scheme responds to the three earlier concerns of Council. Vehicular access to the underground residential parking was earlier located on the north of the site (access from the lane), but the applicant was asked to consider relocating the access to Rupert Street. The access has been relocated, thereby eliminating the prospect of any significant traffic in the adjoining lane to the north.

6

Clause No. 2 Continued

Fire truck access along the easterly edge of the site has been eliminated. The need for fire protection for those residential buildings will be accommodated through sprinkler systems within the buildings. This will also ensure a better relationship with the single-family dwellings to the east, by maintaining more permanent landscaping between the project and the adjacent single-family dwellings. >

Design measures for noise treatment have been agreed to by the applicant. All residential buildings will maintain standards in accordance with CMC Acoustic Standards.

Urban Design Issues

The Urban Design Panel suggested that the commercial buildings be pushed towards Kingsway to create a more positive streetscape. This design revision is considered unreasonable and would obliterate the view corridor established for this site by Council. This guideline is still relevant and should be met.

As an alternate to deleting the view corridor, it is suggested that low masonry walls with landscaping be placed along the site periphery with special surface treatment for the surface parking areas. The City Engineer's suggestion for only one commercial parking ingress/egress point is supported and would minimize the already difficult Kingsway access problems. Both design revisions would be dealt with at the development permit stage and will be noted as specific Council resolutions in the agenda package.

Traffic and Noise

The environmental effects of heavy traffic along Kingsway, a major arterial, and Rupert street, a minor arterial, place constraints on new development. The Guidelines called for noise abatement with commercial uses along Kingsway which would buffer the residential uses. The commercial and residential components have been effectively sited to create the desired noise shield from the impact of Kingsway. The shielding effect of the commercial building is handled more effectively with the present scheme.

Traffic circulation for the residential component is served more effectively with the Rupert Street ingress/egress point.

CONCLUSION

This rezoning application and scheme for a mixed residential/commercial development comprising 45 stacked townhouse units and 1 311.75 m<sup>2</sup> (14,120 sq. ft.) of commercial floor space meets the Kingsway Task Force Guidelines and addresses satisfactorily the concerns expressed by Council in April, 1982 respecting the previous application and scheme, including vehicle access, noise abatement and landscaping, and is therefore supported.

RECOMMENDATION

The Director of Planning recommends that the following be received and referred directly to a Public Hearing:

That the rezoning of 3003 Kingsway from RS-1 and C-2 to CD-1 to accommodate a mixed-use commercial/residential development, be approved, in accordance with the design standards submitted by the applicant. "

The City Manager RECOMMENDS that the recommendation of the Director of Planning be approved.

APPROVED. COUNCIL, July 24, 1984.

7

City of Vancouver



PLANNING DEPARTMENT, 453 WEST 12TH AVENUE, CITY HALL, VANCOUVER, BRITISH COLUMBIA, CANADA V6Z 1K6

Re: Sign By-law

1984 08 09

NOTICE OF PUBLIC HEARING

Amendment to Zoning and Development By-law, No. 3575 and Sign By-law, No. 4810

Dear Sir and/or Madam:

Proposed Rezoning of 3003 Kingsway (Balance of Block 14, D.L. 37, Plan 610A) from RS-1 One-Family Dwelling District and C-2 Commercial District to CD-1 Comprehensive Development District (See Map on Reverse)

On THURSDAY, AUGUST 30, 1984, commencing at 7:30 p.m. in COUNCIL CHAMBERS, THIRD FLOOR, CITY HALL, 453 WEST 12TH AVENUE, the Council of the City of Vancouver will hold a PUBLIC HEARING, pursuant to the provisions of the Vancouver Charter, to consider the above-noted and other proposed amendments to Zoning and Development By-law, No. 3575 and Sign By-law, No. 4810.

If approved, the CD-1 By-law would permit the use and development of the site generally as follows:

- general business commercial uses requiring central locations to serve large neighbourhoods and a maximum of 45 townhouses and apartments;
- a maximum floor space ratio of 0.80 comprising a maximum of 0.20 for commercial uses and 0.60 for residential uses;
- a maximum site coverage of 48 percent;
- a maximum height of 12.192 m (40.00 ft.); and
- provisions regarding off-street parking.

At the PUBLIC HEARING, you or your representative or any persons who deem themselves affected by the proposed By-law amendments shall be afforded an opportunity to be heard on matters contained in the draft By-laws.

The draft By-laws may be inspected in the Zoning Division of the Planning Department, Third Floor, East Wing, City Hall, and in the Office of the City Clerk, Third Floor, City Hall, 453 West 12th Avenue Monday to Friday, from 8:30 a.m. to 5:30 p.m. on regular working days, on and after Tuesday, August 21, 1984. Plans submitted with the application are available for viewing in the Zoning Division.

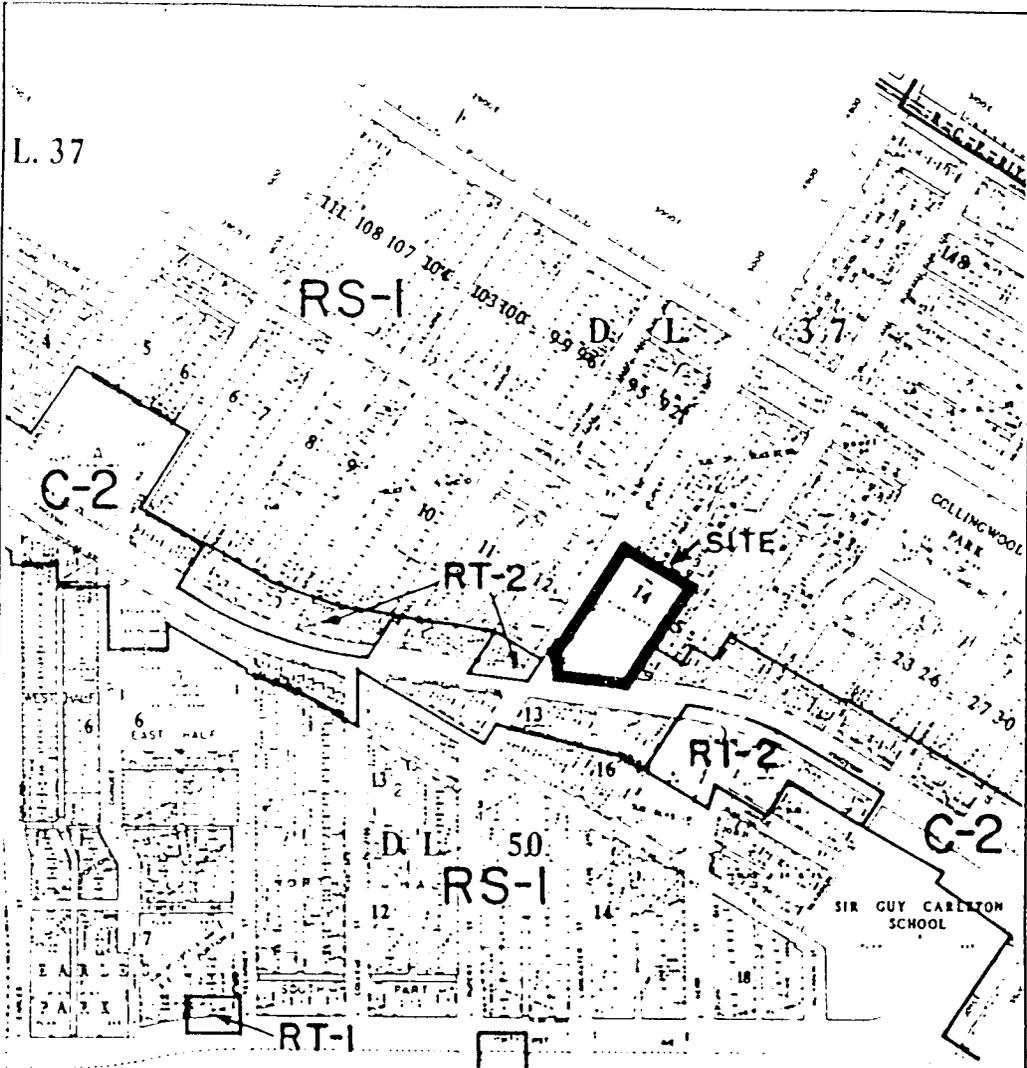
Yours truly,

A handwritten signature in cursive script, appearing to read "D. N. McDonald".

D. N. McDonald  
Associate Director  
Zoning Division

DANW

L. 37



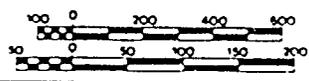
PROPOSED REZONING FROM RS-1 ONE FAMILY DWELLING DISTRICT AND C-2 COMMERCIAL DISTRICT TO CD-1 COMPREHENSIVE DEVELOPMENT DISTRICT



City of Vancouver  
Planning Department

SCALE

Feet  
Metres



CITY OF VANCOUVER  
MEMORANDUM

*Kingway*

From: CITY CLERK

Date: September 5, 1984

To: City Manager  
Director of Planning  
Director of Legal Services  
Associate Director - Zoning  
City Engineer

SEP 06 1984  
FS/34

Refer File: P.H.156

Subject: Public Hearing - August 30, 1984

I wish to advise you of the attached minutes from the Special Council Meeting (Public Hearing) held on August 30, 1984.

Please note any matters contained therein for your attention.

*Handwritten signature*  
CITY CLERK

MK:ss

Also Sent To: Messrs. Neale & Staniszki, Architects,  
#503-321 Water Street, Vancouver B.C.  
V6B 1B8

Mr. G. G. Flanagan,  
Director, Property Development,  
B.C. Transit,  
#301 - 700 West Pender Street,  
Vancouver, B.C.  
V6C 1G8

CITY OF VANCOUVER

SPECIAL COUNCIL MEETING

A Special Meeting of the Council of the City of Vancouver was held on Thursday, August 30, 1984 in the Council Chamber, Third Floor, City Hall, at approximately 7:30 p.m. for the purpose of holding a Public Hearing to amend the Zoning and Development By-law.

PRESENT: Deputy Mayor Rankin  
Aldermen Brown, Davies, Eriksen, Ford,  
Yee and Yorke

ABSENT: Mayor Harcourt  
Aldermen Bellamy  
Kennedy  
Puil

CLERK TO THE COUNCIL: Mrs. M. Kinsella

COMMITTEE OF THE WHOLE

MOVED by Ald. Brown,  
SECONDED by Ald. Ford,

THAT this Council resolve itself into Committee of the Whole, Deputy Mayor Rankin in the Chair, to consider proposed amendments to the Zoning and Development By-law.

- CARRIED UNANIMOUSLY

1. 3003 Kingsway

The Council considered an application by Mr. Derek Neale, Architect, as follows:

LOCATION: 3003 KINGSWAY (Balance of Block 14, D.L. 37, Plan 630A)

Present Zoning: RS-1 One-Family Dwelling District, and  
C-2 Commercial District

Requested Zoning: CD-1 Comprehensive Development District

(i) The draft CD-1 By-law, if approved, would accommodate the use and development of the site generally as follows:

- general business commercial uses and a maximum of 45 townhouses and apartments;
- a maximum floor space ratio of 0.80 comprising a maximum of 0.20 for commercial uses and 0.60 for residential uses;
- a maximum site coverage of 48 percent;
- a maximum height of 12.192 m (40.00 ft.); and
- provisions regarding off-street parking.

(ii) Amend Sign By-law, No. 4810 to establish sign regulations for the newly created CD-1 District.

(iii) Any consequential amendments.

Cont'd.....

3003 Kingsway (cont'd)

The Director of Planning recommended approval subject to the following conditions proposed for adoption by resolution of Council:

- (a) That the detailed scheme of development in a development permit application be first approved by the Director of Planning, after receiving advice from the Urban Design Panel, having particular regard to the following:
  - the overall design and its relationship to adjacent development;
  - pedestrian circulation within and to/from the site;
  - the accessibility, safety, and security of off-street parking facilities;
  - the provision of only one ingress/egress point for the commercial component;
  - the provision and maintenance of landscaping and surface treatment, with particular regard to the peripheral site treatment which may include low masonry walls with landscaping and special surface treatment for surface parking areas;
  - the provision, location, and screening of garbage facilities; and
  - the provision of suitable outdoor lighting.
- (b) That the approved form of development be generally as presented in the drawings prepared by Derek Neale, Architects stamped "Received, City Planning Department, May 24, 1984" provided that the Director of Planning may allow minor alterations to this approved form of development when approving the detailed scheme of development as outlined in resolution (a) above.
- (c) That subdivision plans showing the dedication of 3.048 m (10.00 ft.) of land along Rupert Street for road widening purposes are registered in the Land Title Office.
- (d) That this site be brought to the attention of the Planning and Development Committee of Council if the proposed development has not been started within one (1) year from the date of enactment of the proposed by-law.

Mr. D. Thomsett, Zoning Division, reviewed the application, including the history of previous rezoning applications.

The Deputy Mayor called for speakers for or against the application and Mr. Oscar Nordvik spoke in support.

MOVED by Ald. Brown,

THAT the application by Mr. Derek Neale, Architect, be approved, subject to the conditions proposed by the Director of Planning and quoted in this Minute of the Public Hearing.

- CARRIED UNANIMOUSLY

The Deputy Mayor directed the City Engineer determine and review the Joyce Street Area Planning Committee's concerns with the impact of the proposed development on the Kingsway and Rupert intersection and report back, if necessary.